



**URBAN
DESIGN
PARTNERS**

1213 w morehead st ste 450
charlotte, nc 28208
p 704.334.3303
urbandesigndesignpartners.com
nc firm no: P-0418 sc coa no: C-03044

PDC Land Acquisition, LLC

3715 Northside Pkwy
Building 200 Suite 175
Atlanta, GA 30327

Rhyne Station Road

Development Notes & Standards
Charlotte, NC

NO. DATE: BY: REVISIONS:

Project No: 21-CLT-054
Date: 06.07.2021
Designed By: UJP
Checked By: BDS
Sheet No:

RZ-2.0

Site Development Data:

Acreage: +/- 36.23 AC
Tax Parcels: 055-081-20, 055-091-06 & 055-091-07
Existing Zoning: MX-2 & R-4
Proposed Zoning: R-12MF(CD) & Neighborhood Services (NS)
Existing Uses: Vacant
Proposed Uses: Up to (100) Single-Family Attached Residential Units
Up to (304) Multi-Family Residential Units
2,750SF of Commercial Uses

General Provisions:

- These Development Standards form part of the Rezoning Plan associated with the Rezoning Petition filed by PDC Land Acquisition, LLC (the "Petitioner") to accommodate the development of a mix of single-family attached townhomes, multi-family residential units and commercial uses on an approximate 36.23 acre site located to the north of Old Moores Chapel Road and East of I-485, which is more particularly depicted on the Rezoning Plan (the "Site"). The Site is comprised of Tax Parcel Numbers 055-081-20, 055-091-06 & 055-091-07.
- Development of the Site shall be governed by the Rezoning Plan, these Development Standards and the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance").
- Unless the Rezoning Plan or these Development Standards establish more stringent standards, the regulations established under the Ordinance for the "R-12MF" and "NS" Zoning District shall govern all development taking place on the Site.
- The development and street layout depicted on the Rezoning Plan are schematic in nature and are intended to depict the general arrangement of such uses and improvements on the Site. Accordingly, the final layout, location and sizes of the development and site elements depicted on the Rezoning Plan are graphic representation of the proposed development and site elements, and they may be altered or modified in accordance with the setback, yard(s), landscaping and tree save requirements set forth on the Rezoning Plan and the Development Standards, provided, however, that any such alterations and modification shall be minor in nature and not materially change the overall design intent depicted on the Rezoning Plan.
- Future amendments to the Rezoning Plan and/or these Development Standards may be applied for by the then owner(s) of the Site in accordance with the provisions of Chapter 6 of the Zoning Ordinance. Minor alterations to the Rezoning Plan are subject to Section 6.207 of the Ordinance.

Permitted Uses:

1. Uses allowed within the rezoning area included in this Petition are those uses that are permitted within the R-12MF and NS zoning district, in areas as indicated on the Rezoning Plan, together with any incidental or accessory uses associated with except for the following:

- Automobile service stations
- Convenience stores and gasoline sales
- Car washes
- Automobile drive through windows

Transportation:

- Vehicular access to public rights of way will be as generally depicted on the Rezoning Plan. Final locations, placements and configurations of the vehicular access points shown on the Rezoning Plan are subject to minor modification required to accommodate final permitting and construction plan adjustments as required by NCDOT and CDOT for approval.
- The site shall be served by a combination of public roads and private alleys as depicted on the Rezoning Plan. Final locations of these drives are subject to minor modifications and adjustments to accommodate final permitting and construction plans as required to obtain final approval.
- The petitioner agrees to provide accessible sidewalk ramps at each corner of Public Road 1 and Rhyne Station Road Extension as generally depicted on the Rezoning Plan. The petitioner shall also provide sidewalk and planting strip along Old Moores Chapel Road and existing Rhyne Station Road adjacent The Site. As illustrated. The sidewalk may meander adjacent the property and final location shall be coordinated with NCDOT and CDOT.
- The petitioner shall commit to the following transportation improvements, in coordination with NCDOT and CDOT:
 - Reserved
 - Reserved
- The Petitioner agrees to install Public Road 1 and Rhyne Station Road Extension prior to the sites first certificate of occupancy being issued.
- Installation of the road wearing/surface course for Public Road 1 and Rhyne Station Road Extension must be completed prior to first certificate of occupancy being issued associated with the final 80% of the single family attached townhome units on site.
- The petitioner agrees to install an 8' sidewalk and minimum 8' wide landscape strip adjacent Public Road 1, Old Moores Chapel Road adjacent the site, Rhyne Station Road and Rhyne Station Road Extension. A sidewalk utility easement (SUE) shall be provided between the proposed right of way to 2' behind proposed sidewalk along all internal public roads as generally depicted on the Rezoning Plan.
- Dedication of noted rights of way within the Site Plan shall be conveyed fee simple prior to the site's first certificate of occupancy being issued.

Architectural and Design Standards:

- In addition to design provisions contained within the district regulations of the Zoning Ordinance for the R-12MF district, the development of the site will be governed by the following provisions and standards produced by the Petitioner and which will be binding on the development of the site.
- The petitioner shall limit the base maximum average height of each residential structure on site to 40' and three (3) stories as further described in the Ordinance.
- A variety of principal building materials may be utilized on site and will be a combination of the following: Masonry, brick, concrete, pre-cast concrete, stone, precast stone, pre-finished metal, aluminum, steel, stucco, wood, ceramic tile, cementitious fiber board and glass fiber reinforced concrete. Vinyl, as a building material, will only be allowed on windows, soffits and trim features
- Building placement and site design of the multi-family apartment buildings on site shall focus on and enhance the pedestrian environment adjacent public streets.
 - Buildings shall be placed so as to present a front or side façade to all public streets.
 - Usable porches or stoops may form an element of the building design and be located on the front and/or side of a building or residential unit.
 - Direct pedestrian connections should be provided between residential units adjacent public streets.
 - The facades of first/ground floor of the buildings along public streets shall incorporate a minimum of 30% masonry materials such as brick or stone.
 - Building elevations shall not have expanses of blank walls greater than 20 feet in all direction and architectural features such as but not limited to banding, medallions or design features or materials will be provided to avoid a sterile, unarticulated blank treatment of such walls.
 - Building elevations shall be designed with vertical bays or articulated architectural features which shall include a combination of at least three of the following: a combination of exterior wall offsets (projections and recesses), columns, pilasters, change in materials or colors, awnings, arcades, or other architectural elements.
- Placement and design of single-family attached townhome residential units on site shall adhere to the following guidelines to enhance the architectural character or the proposed structures.
 - Pitched roofs shall be symmetrically sloped no less than 5:12 except for roofs for porches that shall be no less than 2:12.
 - Porches and stoops shall form a predominant feature of the building design and be located on the front of the building when adjacent a public right of way. Usable front porches may be covered but not enclosed.
 - Walkways shall be provided to connect all residential entrances to sidewalks along public streets.
 - All HVAC and mechanical equipment shall be screened from public view.
 - Attached dwelling units shall be limited to a maximum of six (6) townhome units per building.
 - Roof overhangs, eaves, cornices, chimneys, gutter, vents, bay windows, pilasters, pillars, open porches (if provided), and other architectural elements may project up to (24) inches into the required setbacks.
- Building placement and site design of the non-residential building proposed within the NS district on site shall focus on and enhance the pedestrian environment along Rhyne Station Road Extension.
 - The building shall be placed so as to present a front or side façade to all streets.
 - Direct pedestrian connections should be provided between street facing doors and corner entrance features to sidewalk on adjacent streets
 - Building elevations shall not have expanses of blank walls greater than 20 feet in all direction and architectural features such as but not limited to banding, medallions or design features or materials will be provided to avoid a sterile, unarticulated blank treatment of such walls.
- The Site shall comply with Section 12.403 of the Zoning Ordinance. Dumpster and recycling service areas associated with residential uses shall be as generally depicted on the Site plan and shall be screened from public view and described within the ordinance. Final locations of dumpster locations shall be determined during the land development review permit process.

Streetscape and Landscaping:

1. The Petitioner shall comply with Chapter 21 of the City of Charlotte Code of Ordinances.

Environmental Features:

- The Petitioner shall comply with the City of Charlotte Post Construction Ordinance.
- The location, size, and type of storm water management systems depicted on the Rezoning Plan are subject to review and approval as part of the full development plan submittal and are not implicitly approved with this rezoning. Adjustments may be necessary in order to accommodate actual storm water treatment requirements and natural site discharge points.
- Development within any SWM/PCSO Buffer shall be coordinated with and subject to approval by Charlotte-Mecklenburg Storm Water Services as mitigated if required by City ordinance. Petitioner acknowledges intermittent/perennial stream delineation reports are subject to review and approval upon submission of development plans for permitting and are not approved with rezoning decisions.

Signage:

1. The Petitioner shall comply with the City of Charlotte Signage Ordinance.

Lighting:

- All attached and detached lighting will be full cutoff fixtures and downwardly directed. However, upward facing architectural and landscape accent lighting shall be permitted.
- Detached lighting on the site, except street lights located along public streets, will be limited to twenty-one (21) feet in height.

Amendments to Rezoning Plan:

Future amendments to these Development Standards may be applied for by the then Owner or Owners of the parcel or parcels within the Site involved in accordance with the provisions of Chapter 6 of the Ordinance.

Further alterations or modifications to the Rezoning Plan which, in the opinion of the Planning Director, substantially alter the character of the development or significantly alter the approved Technical Data Sheet or any of its conditions or which increase the intensity of development shall not be deemed to be minor and may only be made in accordance with the provisions of Subsections 6.207(1) or (2) of the Ordinance, as applicable.

Binding Effect of the Rezoning Documents and Definitions:

- If this Site Plan Amendment is approved, all conditions applicable to development of the Site imposed under the Technical Data Sheet will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of the Petitioner and the current and subsequent owners of the Site and their respective successors in interest and assigns.
- Throughout these Development Standards, the terms, "Petitioner" and "owner" and "owners" shall be deemed to include the heirs, devisees, personal representatives, successors in interest and assigns of the Petitioner or the owners of the Site from time to time who may be involved in any future development thereof.